

LA VISTA CITY CENTRE REDEVELOPMENT AREA DESIGN GUIDELINE

City of La Vista, Nebraska



Exhibit "G"

La Vista City Hall
8116 Park View Boulevard
La Vista, Nebraska
2 August 2016
Amended: 19 March, 2024



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1. INTRODUCTION

This design guideline provides a framework within which to design and review projects located in the La Vista City Centre Redevelopment Area, and gives direction to designers and developers in understanding the City's expectations for urban style development. This manual is intended to provide a degree of predictability, harmony and quality within the built environment.

The City of La Vista understands the value of aesthetics and amenities as vital ingredients in strengthening and enhancing community identity; establishing and maintaining economic value; and implementing the City's long-range vision to turn the 84th Street area into an urban mixed use corridor. These standards and guidelines were developed as a means of promoting consistent, quality development within the areas identified in the La Vista City Centre Redevelopment Area.

The criteria contained herein are not intended to restrict imagination, innovation, or variety, but rather to assist in focusing on design principles that can result in creative solutions that will develop a satisfactory visual appearance within the city, preserve taxable values, and promote the public health, safety, and welfare.

This design guideline is to be used by property owners/developers and their design consultants in the planning of development in the La Vista City Centre Redevelopment Area. The design guideline will also be used by City staff, the Planning Commission and the City Council as part of their review of development proposals.

2. GEOGRAPHIC AREA AND CRITERIA

It is the intent of the City for this Design Guideline to apply to all property within the La Vista City Centre Redevelopment Area.

New construction and modifications to existing buildings, including the structure and the surrounding property, are required to have compliance reviewed through the design review process.

Exceptions:

Conformance to this Design Guideline shall not apply if the project consists of one of the following:

- 1) Structural modification which will not be visible from outside the structure.
- 2) Container bars that have been approved through the Conditional Use Permit process.

3. Architecture

It is the intent of the City of La Vista, through the La Vista City Centre Redevelopment Area Design Guidelines, to allow for and encourage the creation of an attractive, active and distinct urban district along the 84th Street corridor. The disposition, function and design of buildings plays an important role in achieving that intent. The architecture of buildings assists in the creation of pedestrian-friendly places that are a key part of the corridor areas. Buildings should provide a sense of proportion, stability, and visual balance by establishing a clear expression of base, middle, and top.

Tripartite Architecture

All buildings shall be designed and constructed in tripartite architecture so that they have a distinct base, middle, and top.

Guideline: New construction shall respect the vertical architectural composition through tripartite expression of base, which relates to the scale of the pedestrian environment, a middle, which contains a pattern of fenestration and detail, and a top, which relates to the scale of the skyline.

A change in material between the base and the middle floors, and the use of a significant cornice provides for tripartite architecture.



A significant amount of glass is provided at the base of this building. The base also reflects a greater height than any single floor in the middle. Note the step back on the top floor.



360-Degree Architecture

A building's special architectural features and treatments shall not be limited to a single façade. All sides of a building open to view by the public, whether viewed from public or private property, shall display a similar level of quality and architectural interest.

Guideline: Architectural features such as windows, awnings, projections, reveals, changes in pattern, and trellises shall be used on all sides for visual interest. The dimensions of base, middle, and top shall be carried around from the primary facades to the side and rear of the building.

The same level of architectural quality and articulation has been provided on all four elevations of this building.



First Floor Façade Treatment

First floor facades of buildings within the redevelopment area that face public or private streets, plazas, or open space shall exhibit high levels of design, detailing, and material quality.

All buildings fronting on a street shall be designed so that the first floor street façade includes clear glass windows and doors arranged so that the uses are visible from and to the street on at least 50 percent of the façade.

Guideline: The first floor of all buildings shall provide for a pedestrian-friendly environment, with human-scale and natural building materials; extensive storefront windows for display and views into the business; and access directly from adjacent sidewalks. When transparency is in conflict with internal functions of the building, other means should be used to activate the street facing facades such as public art, architectural ornamentation or details, or color patterns.

The first floor consists almost entirely of glass providing a view into the restaurant space and also providing space for interaction through open overhead doorways. Additionally, the main entrance is provided directly from the street.



Scaling Elements

Architectural scaling elements, such as banding, belt coursing, sills, lintels, mullions, and changes in texture, material module and pattern, shall be used to break down the appearance of large building forms. Horizontal and/or vertical variation should be used.

Guideline: Building facades shall include a combination of details to enhance the architectural interest. For example, use brickwork to create unique elements, or mix materials of varying depth to provide visual interest.

This building utilizes banding, belt coursing, mullions, and changes in material and pattern to break down the appearance.



This photo illustrates the use of brick patterns to create interest.



Pedestrian-Scale Construction Materials

To promote a sense of human scale, special accent materials and design details shall be incorporated into all first floor facades and paving areas abutting pedestrian walkways.

Guideline: First floor facades and building entrances shall include changes in materials, decorative wall patterns, and/or reveals. Paved areas at building entrances should include changes in pattern or color.

This façade includes human-scale material on the bottom floor. Note the change in the paving pattern at the building entrances.



These facades use awnings in combination with streetscape elements to provide an inviting pedestrian environment.



Articulated Facades

Exterior walls greater than 40 feet in length shall break any flat, monolithic façade with discernible architectural elements. Building designs, rooflines, or façade treatments that are monotonous will not be allowed.

Guideline: Building facades oriented to the street or public space shall provide architectural variety and scale by incorporating elements such as bay windows, recessed entrances and windows, display windows, balconies, cornices, columns, vertical plane breaks, and other types of architectural detailing to provide visual interest.

Articulated walls, columns, changes in roof height, and pedestrian-scale features have been used on this apartment building to break up the large mass of the building.



As above, this building utilizes articulated walls, columns, changes in roof height. Yet it also utilizes shade structures, balconies and awnings to help break up the mass.



Parking Structure Design

The first floor façade of structured parking facilities shall be designed to encourage and complement pedestrian-scale interest and activity.

Structured parking facilities shall be designed so that vehicles parked on all levels of the facility are screened from public view.

Guideline: The ground floor of parking structures located adjacent to major public streets should include a use other than parking, such as retail or office. Such a mix of uses is required in the redevelopment area. Parking can also be wrapped by development as a screening device.

This parking structure effectively screens vehicles from public view and utilizes a number of architectural aspects to break up the façade and to provide a pedestrian scale on the ground floor.



This parking structure includes ground floor retail and restaurant space. Additionally, parked vehicles are screened from public view.



Sloped Roof Characteristics

Sloped roofs are not an allowed roof type.

Guideline: Unless used as an accent feature to a flat-roofed building, sloped roofs are not allowed.

Flat Roof Characteristics

Buildings shall create varied parapet and cornice lines in order to create interesting skylines.

Guideline: Design elements for flat roof buildings shall include parapets with variable height and/or changes in setback. Where possible, rooftop areas are encouraged to be used for public or private outdoor space.

These buildings incorporate changing parapet heights and plane breaks to enhance the flat roof design.



Internal Circulation

All stairwells, corridors, and circulation components of the building shall be completely enclosed within the building envelope.

Guideline: Stairs and other circulation components shall be located within the building envelope. However, such elements can still be visible through the use of glass for pedestrian safety.

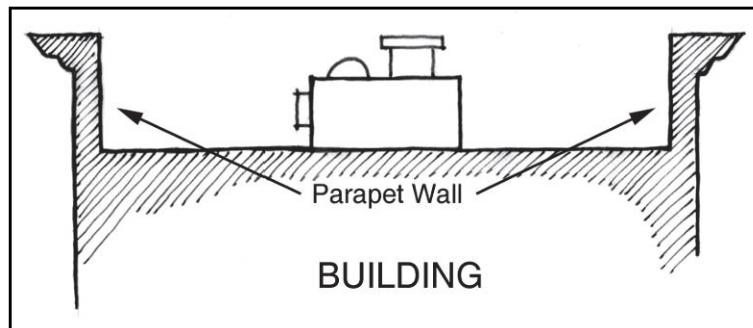
These parking structures include stairways completely enclosed with glass. This element provides for visual interest, as well as pedestrian safety.



Screening of Roof-Top Equipment

All rooftop mechanical equipment shall be screened from public view through the use of parapets or enclosures that are equal to or greater than the height of the equipment to be screened. The parapet or enclosure shall be compatible with the overall architectural character and scale of the building.

Guideline: Mechanical screening techniques shall be used to provide additional visual interest at the roof level. Review of screening shall be conducted through an elevation view cross section.



Use of Bright or Intense Colors

Intense, bright, or fluorescent colors should not be used as the predominant color on any wall or roof of any primary or accessory structure.

Guideline: These colors may be used as building accent colors, but should generally not constitute more than 10 percent of the area of each elevation of a building.

These two buildings include bright colors that highlight architectural articulation.



Exterior Building Materials

Exterior building facades shall exhibit high levels of design, detailing, and material quality. A mix of high quality, compatible materials is strongly encouraged on all facades facing streets or other public spaces or areas.

Guideline: Buildings shall be constructed of durable, high-quality materials such as: brick, stone, architectural pre-cast concrete, architecturally cast concrete, cast stone, integrally colored split or ground face concrete masonry units, terra-cotta, stucco or EIFS (exterior insulated finishing system), architectural metal, integrally colored cement board siding, integrally colored composite rain screen panels, or any combination of the materials listed.

These buildings utilize a combination of a number of high-quality materials and detail elements.





Design of Medium and Large Format Retail Buildings

At least one side of the building shall be located adjacent to a public street and meet the setback requirements provided in the Mixed Use – City Centre zoning district.

All façades located adjacent to a public street shall be, or appear to be, a minimum of two stories in height. The second story façade shall, at a minimum, include spandrel glass windows with architecturally appropriate sills, trim and mullions.

Ground floor facades adjacent to public streets shall have display windows, entry areas, awnings, and other similar pedestrian-oriented design elements along no less than 70 percent of the façade length.

Guideline: Medium to large format, or “big box” retail buildings, (7,000+ sqft) shall respect the pedestrian environment. The buildings shall contain architectural details consistent with pedestrian-friendly building design.

This large format retail building integrates well with the surrounding two story buildings.



This single-story large format retail building appears to be multi-story, is located adjacent to the street, and has facades that contain display windows and pedestrian-scale construction material.



The look of this large format retail building is broken up by the smaller format retail buildings in front of it.



4. Urban Design

Urban design encompasses the various ways that buildings and development interact with the public realm. The intent of urban design as it relates to the Mixed Use – City Centre district is to create a pedestrian-friendly environment that connects a mix of land use types to one another, as well as to other parts of the community, including Civic Center Park.

Drive-Through Windows

Drive-through windows shall not face public streets. Drive-through lanes shall not be allowed in the area between a building and a public street.

Guideline: Drive-through windows and drive lanes shall have a minimal impact on the pedestrian environment. Windows shall be located on the side or rear of a building, or hidden from the street by an extension of the building or low screen wall. Drive-through lanes shall be located on no more than two sides of a building.

Building Step-Backs

Building step-backs are permitted above the second floor of any building or structure.

Guideline: Building step-backs should be used to create visual interest in the building, and to provide sunlight to the pedestrian environment where appropriate.

This building includes step-backs at various levels.



This building incorporates numerous step-backs.

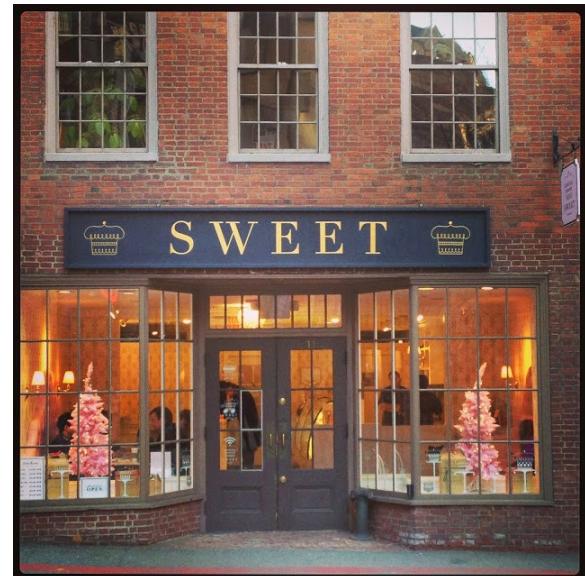


Building Entrances

All buildings and ground floor users shall provide a primary entrance that either faces an adjacent public or private street or is placed at an angle of up to 45 degrees from an adjacent street, measured from the street property line.

Guideline: Building entries shall be designed to encourage pedestrian activity along street frontages rather than within parking lots.

Building entrances for office and retail uses should have direct interaction with the street frontages, rather than parking areas, to encourage pedestrian traffic.



This building has a corner entrance, drawing pedestrian traffic from both streets.



Parking Lot Location/Screening

All surface off-street parking lots shall be primarily located behind buildings that face on a public street and be accessed by an alley or short driveway located between buildings. Where surface parking is located adjacent to a public or private street, landscaping or a low screen wall providing screening to a height of 36 inches shall be provided.

Guideline: Pedestrian interaction with parked vehicles shall be minimized to the greatest extent possible with parking lots located behind buildings. The use of high quality masonry walls and/or shrub landscaping provides the most appropriate screening when the parking lot does abut the sidewalk.

These are examples of a mixture of vegetative screening for surface parking lots. Note that the mixture provides both screening at various heights.



Screening of Service Areas

Loading docks and all other service areas shall be fully screened from view by walls.

Guideline: Loading and service areas should not be visible from any areas primarily used by the public. Loading areas should be concentrated in common courts to minimize visual impacts.

Guideline: Roof structures should be used to screen docks and trash enclosures. The roof structures should match the materials and colors prevalent on the primary building to which it is attached.

This service area has been located completely within the building it serves.



5. Sidewalks and Plazas

Sidewalks and plazas are key features in pedestrian-friendly, urban environments. Sidewalks, separated from auto traffic lanes by street trees and tree lawns, should connect the surrounding uses. Plazas and public open spaces should be used to create nodes of pedestrian activity.

Sidewalk Design Adjacent to Local and Private Streets

Sidewalks adjacent to local and private streets shall adhere to the Master Streetscape Plan, attached as Exhibit A.

Guideline: Sidewalks adjacent to local and private streets should be designed to be as pedestrian-friendly as possible, through the use of landscape materials between the sidewalk and back-of-curb.

Open Space and Plaza Design

Open space within the La Vista City Centre Redevelopment Area is required to be provided as public plazas, pocket parks, roof top gardens, or courtyards.

All open space shall be accessible to users of the building(s) and be improved with seating, plantings, and amenities. Open space areas should be visible from adjacent streets or pedestrian areas to the greatest extent possible.

Guideline: Open space should be used as an urban design element wherever possible. Open space and plaza areas shall contain a mix of pedestrian amenities, such as water features, benches, and shade structures.

This plaza utilizes abundant seating, along with a mixture of levels of shade, while preserving the view.



The next two plazas utilize unique seating, lighting, and shade structures.



6. Connectivity

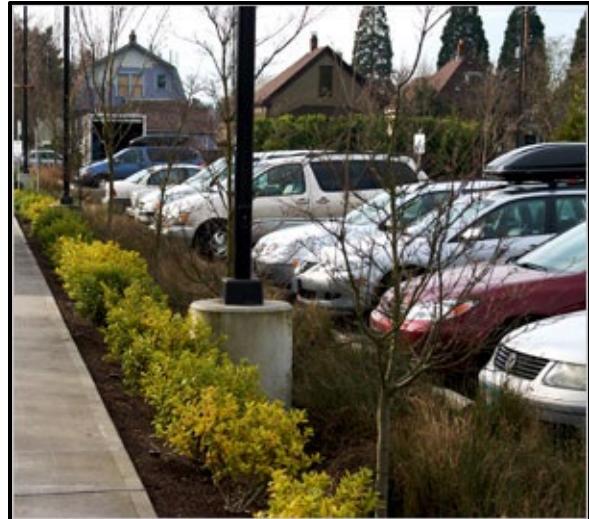
Development within the La Vista City Centre Redevelopment Area shall be integrated with the surrounding community, be easily accessible and have a coherent and well-designed internal circulation system for a variety of travel options. Connectivity shall take in to account pedestrians and cyclists, and minimize the impact of the automobile wherever possible.

Pedestrian Ways Through Parking Lots

Wherever possible, sidewalks through surface parking areas should be located within landscape islands. In any case, each point at which the system of sidewalks must cross a parking lot or internal street or driveway to make a required connection shall be clearly marked through the use of a change in paving materials, height, or distinctive color.

Guideline: Pedestrian walkways should be separated from vehicle drive lanes wherever possible. Landscaping should be used to buffer pedestrians from motor vehicles. Where pedestrians must cross drive lanes, it should be clear that they have priority.

This photo illustrates how pedestrian walkways can be separated from parking areas and drive lanes through the use of landscaping.



This photo illustrates how a change in paving material can clearly mark the pedestrian route through a parking area.



Connections to External Sidewalks and Open Space

External walk connections are required to provide direct access from all buildings on the site to existing or planned sidewalks, adjacent multi-use trails, parks, and greenways.

Guideline: Provide connections to adjacent sidewalks and open space corridors wherever possible. The connections should be direct, have an appropriate width, and be well lit at night.

This walkway provides a direct connection between the parking located behind the buildings and the sidewalk along the storefronts.



This photo illustrates a pedestrian route between buildings that utilizes pavers, plantings, and seating to soften the feel of the route between the buildings.



7. Parking

Parking, and the visual impact of large surface lots, should be minimized within the La Vista City Centre Redevelopment Area. Shared parking shall be permitted and is encouraged.

Bicycle Parking

Bicycle parking is required for development within the La Vista City Centre Redevelopment Area to encourage the use of this mode of transportation.

Guideline: Bicycle parking shall be located in safe and convenient locations adjacent to buildings or in a central location for multi- building developments. Short term bicycle parking should be located no more than 50 feet from the principal building entrance.

This bicycle rack is located adjacent to the building entrance.



This rack is also close to the main entrance.

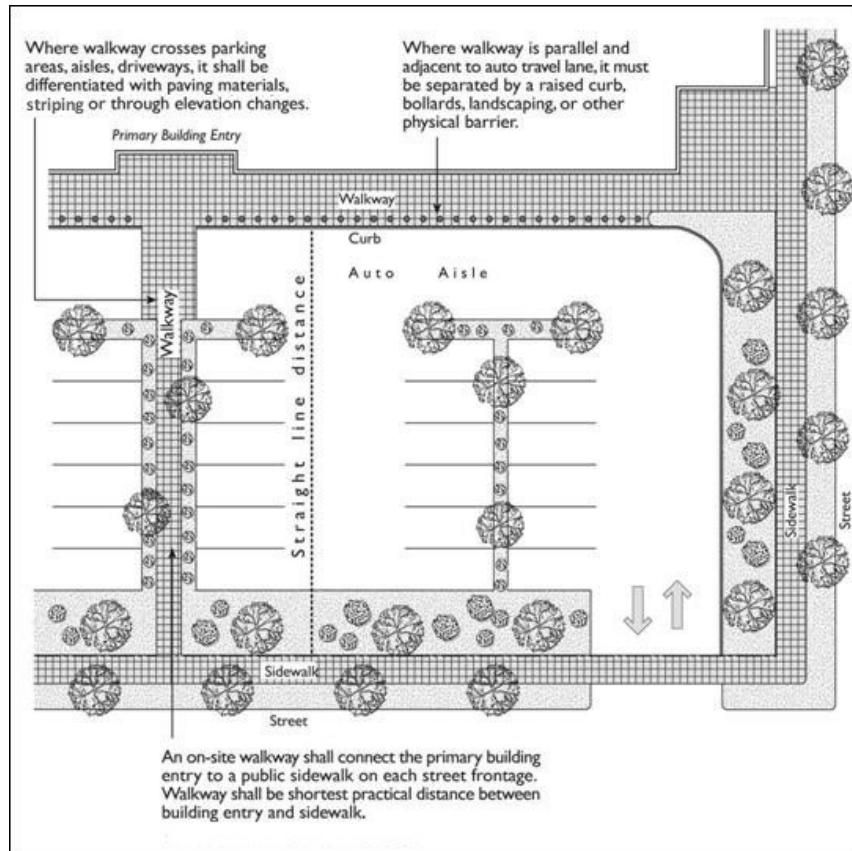


Off-Street Surface Parking

All surface off-street parking should be primarily located behind buildings that face on a public street and be accessed by an alley or short driveway located between buildings. Pedestrian paths shall be clearly delimitated through striping or changes in color or material type.

At least 10 percent of the area of surface parking lots shall include trees and shrubs.

Guideline: Off-street parking areas shall be screened from the view of public streets to the greatest extent possible. Surface parking areas shall use landscaping and pedestrian walkways to divide the lot into smaller modules.



This illustration indicates an appropriate parking lot configuration.

On-Street Parking

On-street parking shall be provided where permitted by the City of La Vista.

Guideline: On-street parking shall be provided wherever possible. On-street parking provides a buffer between pedestrians and through traffic lanes and indicates activity, while also providing convenient parking directly in front of businesses.

On-street parking provides convenient short-term opportunities directly in front of businesses.



On-street parking should always be provided in residential areas for convenient guest parking.



8. Signs

Signs along commercial frontages shall be clear, informative to the public and durable. Signs shall be scaled to the pedestrian-oriented nature of the La Vista City Centre Redevelopment Area.

Wall Signs

Wall signs are permitted within the area between the second floor line and the first floor ceiling, within a horizontal band not to exceed 42 inches in height. The horizontal band shall be between 12 and 18 feet above the adjacent sidewalk. The total length of wall signage shall not exceed 70 percent of the frontage associated with the use.

Guideline: Wall signs shall be designed to complement the architecture to which they are attached. Signs shall reflect the scale of the building, while also creatively identifying the business.

These are examples of appropriately scaled and designed wall signs.



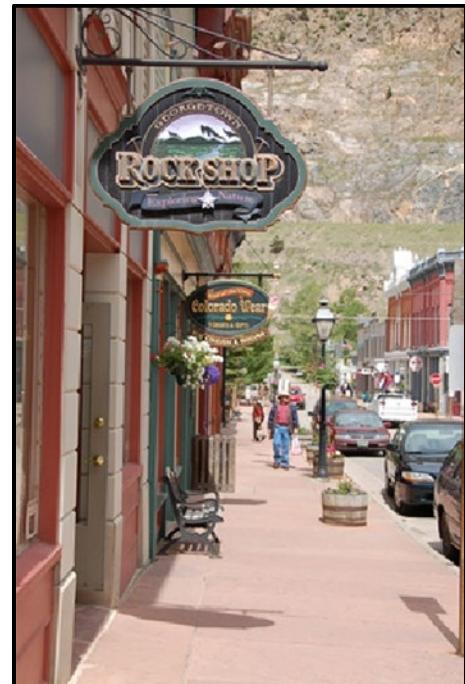


Projecting Signs

Each use in a building shall be allowed one projecting sign for each street frontage. The sign shall not exceed 12 square feet per face, not project more than four feet, and have a minimum clearance of 10 feet above the adjacent sidewalk. Projecting signs may include three-dimensional logos.

Guideline: Projecting signs create better visibility for pedestrians on the sidewalk than other sign types. This type of sign should be encouraged throughout the La Vista City Centre Redevelopment Area.

The variety of design in the projecting signs provides visual interest.



These projecting signs incorporate interesting brackets.





Awning Signs

Each use shall be allowed one sign per awning associated with the use. Signage is allowed only on the vertical front portion of the awning, except that graphical logos shall be allowed on the slanted portion. Letters shall not exceed 8 inches in height, and logos shall not exceed 10 percent of the sloped awning panel area.

Guideline: Awning signs should be encouraged for all retail spaces. Awning signs should be unique to each business or use.

These images of awnings identify the business, as well as the services and products provided.



Monument Signs

Monument signs shall be compatible with the architecture of the building to which it is associated.

Guideline: The use of monument signs should be limited to major arterial streets. However, when they are utilized they should be low profile and be compatible with the architecture of the buildings to which they are associated.

9. Lighting

Vehicular and pedestrian lighting shall be provided throughout all vehicular and pedestrian circulation areas to promote safety and walkability.

Pedestrian Lighting

Sidewalks, internal pedestrian paths, and bicycle paths shall be lit with full cutoff lighting fixtures no more than 16 feet tall and providing consistent illumination.

Guideline: Pedestrian lighting shall be human-scaled and also reflect the overall character or design of the project to which it is associated.

Parking Lot Lighting

On-site street and parking areas shall be lit with full cutoff type lighting fixtures with a maximum height of 25 feet.

Guideline: Parking lot lighting should be integrated into landscape islands wherever possible, and should be compatible with the overall design of the associated project.

Building Lighting

Building lighting should be full cutoff fixtures and should reflect the architectural characteristics of the overall building.

Guideline: Building lighting should complement the overall building architecture in design and nighttime illumination. Building lighting should also be used to light pedestrian ways adjacent to the building.

This building uses up-lighting to accentuate the architectural characteristics of the building as well as to light pedestrian entrance points to the building.



This building also uses up-lighting to accentuate the architectural characteristics of the building at night.



This building utilizes up-lighting, under-eave lighting, and clear-story lighting to provide a nighttime setting for the building.



10. Pedestrian Amenities

Public sidewalks and areas within the La Vista City Centre Redevelopment Area shall be enhanced with decorative pavement treatments, ornamental street lighting, streetscape furnishings, and public art as part of the design and experience of the area.

Seating

Convenient and attractive seating shall be provided wherever appropriate to enhance the pedestrian environment and shall adhere to the Master Streetscape Plan, attached as Exhibit A.

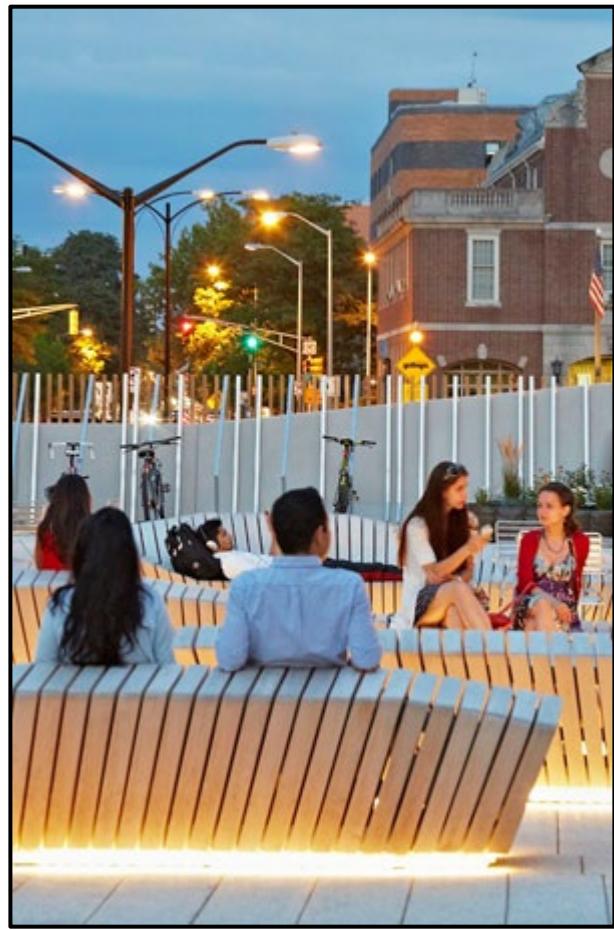
Guideline: Seating areas shall be included as part of the overall development to provide places for pedestrians to rest and “people-watch.”

Example of convenient and attractive seating options.



Examples of clusters of seating in a comfortable atmosphere.





Planters and Tree Grates

Planters, decorative tree grates, and other landscape-associated amenities shall be provided to enhance the pedestrian environment as defined by the Master Streetscape Plan, attached as Exhibit A.

Guideline: Use decorative and landscape-related items to enhance the pedestrian environment, as well as the environments along streets, and adjacent to building and parking areas.

Decorative planters can improve the sidewalk appearance.



An example of a decorative tree grate.



Another example of a decorative tree grate.



Public Art

Public art should be provided to strengthen the community and cultural identity within the City Centre area, while also enhancing the streetscape.

Guideline: Public art should be integrated into the design of transit and parking facilities, streetscape improvements, and outdoor environments associated with new development projects.

This large example of public art is notable and can provide a commonly known point of gathering.



This example of public art also acts as a bike rack.



Here a pedestrian bridge is enhanced with public art.



11. MAINTENANCE—PLANNING AND DESIGN FACTORS

- A.** Continued good appearance depends upon the extent and quality of maintenance. The choice of materials and their use, together with the types of finishes and other protective measures, must be conducive to easy maintenance and upkeep.
- B.** Materials and finishes shall be selected for their durability and wear as well as for their beauty. Proper measures and devices shall be incorporated into the design for protection against the elements, neglect, damage, and abuse.
- C.** If prefinished metal is allowed, TNEMEC coated metal, or approved equal is required.

12. FACTORS FOR EVALUATION

The following factors and characteristics, which affect the appearance of the development, will govern the evaluation of a design submission:

- A.** Conformance to ordinances and this Design Guideline
- B.** Logic of design
- C.** Exterior space utilization
- D.** Architectural character
- E.** Appealing as to form, character, and design
- F.** Material selection
- G.** Harmony and compatibility
- H.** Circulation-vehicular and pedestrian
- I.** Maintenance aspects
- J.** Pedestrian scale

13. RESUBMITTAL REQUIREMENTS

After the initial submittal, digital submissions are acceptable, with the exception of material and color samples. A final hard copy submittal in 11" x 17" format shall be required after final approval.

14. APPROVAL OF CHANGES AFTER DESIGN ACCEPTANCE

It is the owner's responsibility to point out and submit any exterior modifications that are proposed between design acceptance and completion of construction to assure timely issuance of a Certificate of Occupancy.

15. PROCESS

PRE-APPLICATION CONFERENCE:

A pre-application conference with city staff and the city's design review architect is required and gives the applicant an opportunity to discuss plans before a great deal of time or money is expended. If a certain design is inappropriate, the applicant will know after the pre-application conference.

APPLICATION FOR DESIGN REVIEW:

The applicant needs to fill out the "Application for Design Review" and submit it along with the required submittals. A listing of required submittals is included as part of the application form. The application fee required for this submittal shall be in accordance with La Vista's Master Fee Schedule.

DESIGN REVIEW:

The City of La Vista staff in association with the city design review architect will review the submittal documents for compliance with the La Vista City Centre Redevelopment Area Design Guidelines and the pre-application conference.

SCHEDULE OF REVIEWS:

A completed application will take approximately three weeks to review. Incomplete applications may cause a delay. Additional reviews will be necessary for all revised submittals until a Certificate of Approval is issued.

CERTIFICATE OF APPROVAL:

Upon a successful review the City of La Vista will issue to the applicant a Certificate of Approval. A copy of this document will need to be included with the Building Permit documents in order to receive a Building Permit.

APPEALS:

In the event where the applicant and the City cannot come to an agreement within 180 days of initial application submission, the applicant may request a meeting with the City Administrator regarding an appeal to the City Council.

OCCUPANCY PERMIT:

After the building permit is issued, all design requirements must be completed as approved in order for a Certificate of Occupancy to be issued for the building.

MAINTENANCE OF DESIGN REQUIREMENTS:

The applicant needs to maintain the design requirements for the life of the project. In the event that they fail to do so, the City may revoke the Certificate of Occupancy.

Exhibit "A"
Master Streetscape Plan
(To be added prior to recording)